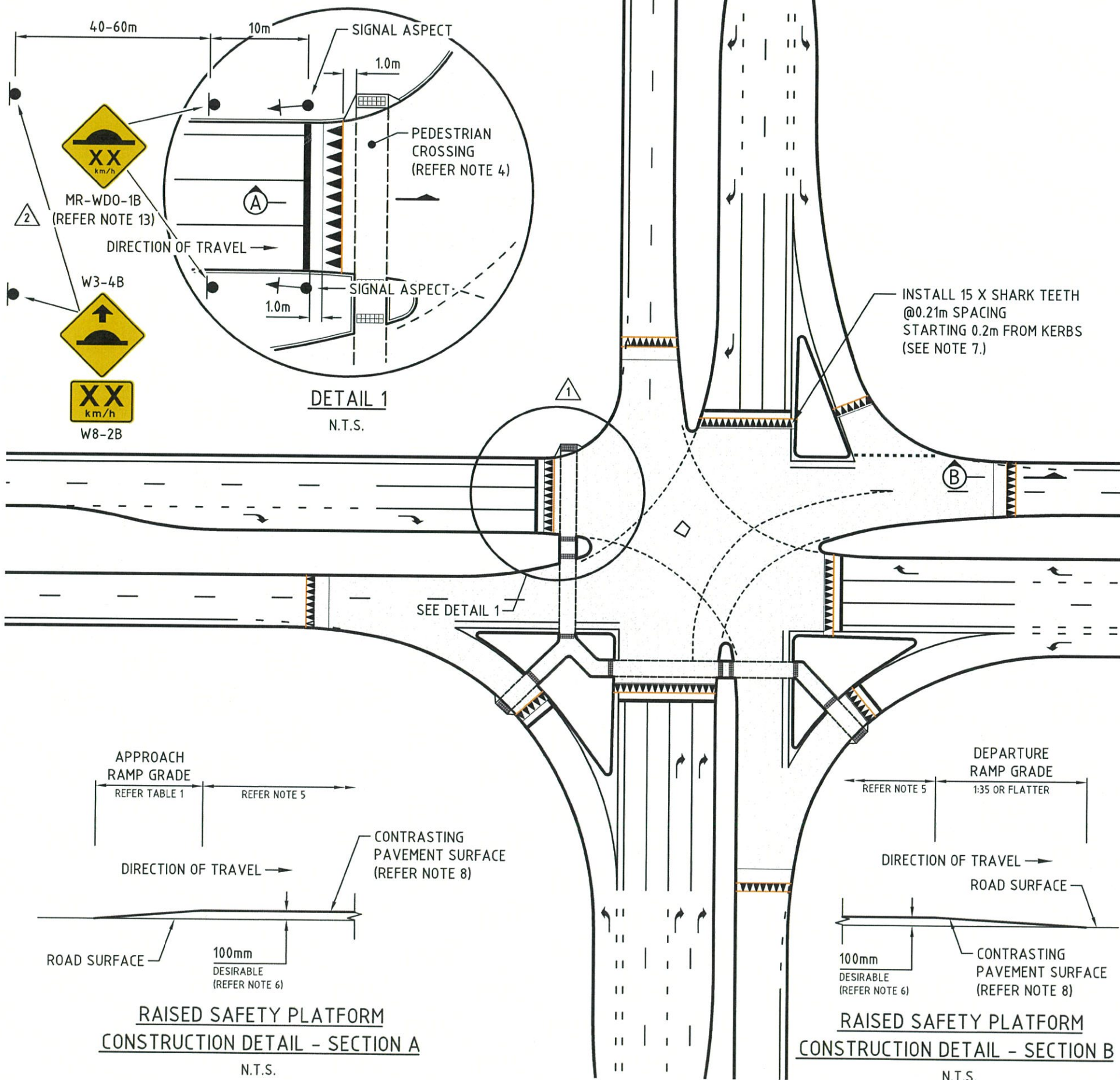


OPERATING SPEED km/h	APPROACH RAMP GRADE	COMFORTABLE MAXIMUM SPEED (km/h)
50	1:15 (6.7%)	30
60	1:20 (5.0%)	40
70	1:25 (4.0%)	50

TABLE 1

NOTES

1. A RAISED SAFETY PLATFORM (RSP) SHOULD BE ORIENTATED PERPENDICULAR TO THE DIRECTION OF TRAFFIC FLOW SO THAT BOTH FRONT WHEELS OF THE VEHICLE COMMENCE TO RISE OR FALL ON THE RAMPS CONCURRENTLY.
2. WHEN INSTALLING A RSP ON THE APPROACH TO AN INTERSECTION, RAMPS MUST BE LOCATED CLEAR OF THE THROUGH LANES OF THE INTERSECTING ROAD.
3. RSP RAMPS MUST BE FLAT WITH A CONSISTENT GRADE BETWEEN THE TOP AND BOTTOM OF THE RAMP.
4. WHERE A RSP IS INSTALLED IN CLOSE PROXIMITY TO PEDESTRIAN FACILITIES, PEDESTRIAN REFUGES MUST BE SET BACK A MINIMUM OF 1.0m FROM THE RAMP TO PREVENT THE POSSIBILITY OF PEDESTRIANS MISTAKING RAMP MARKINGS FOR A CROSSING FACILITY. ALTERNATIVELY, A PEDESTRIAN CROSSING MAY BE INCORPORATED INTO THE RSP TO MINIMISE ANY CONFUSION AND ACHIEVE BETTER PEDESTRIAN SAFETY OUTCOMES.
5. THE FLAT SECTION OF THE RSP IS RECOMMENDED TO BE EXTENDED OVER MORE THAN A STANDARD PASSENGER CAR LENGTH (I.E. AT LEAST 6.0m), INCLUDING WHEN USED AS A PEDESTRIAN CROSSING. LENGTHS OF RAISED INTERSECTION PLATFORMS WILL DEPEND ON THE SIZE AND CONFIGURATION OF THE ENTIRE INTERSECTION.
6. THE TYPICAL RSP DIMENSIONS RECOMMENDED ARE FOR A DESIRABLE HEIGHT OF 100mm. 75mm MAY BE CONSIDERED WHERE SITE CONSTRAINTS AND TRAFFIC COMPOSITION SUGGESTS A LOWER HEIGHT PROFILE (E.G. HIGH TRUCK VOLUME ROUTES). RAMP HEIGHTS LESS THAN 75mm ARE NOT EFFECTIVE AT REDUCING SPEEDS AND SHOULD NOT BE CONSIDERED. 150mm MAY BE USED FOR LOW SPEED (LESS THAN 50km/h) AND LOW TRAFFIC VOLUME ENVIRONMENTS, HOWEVER, PLATFORMS THAT ARE OVER 100mm MAY DAMAGE LOW-FLOOR VEHICLES AND ARE NOT TO BE USED ON ANY ARTERIAL ROADS
7. SHARKS TEETH SHALL BE APPLIED ONTO THE RAMP AND NOT ONTO THE PRECEDING ASPHALT. SHARKS TEETH SHALL EXTEND TO 0.2m FROM KERBS. REFER MRWA STANDARD DRAWING 202131-0030.
8. THE RAMP SHALL NOT BE CONSTRUCTED FROM BRICK PAVING SINCE THIS HAS POOR ADHESION FOR ROAD MARKINGS AND SHOULD BE CONSTRUCTED FROM A MATERIAL WITH A CONTRASTING COLOUR TO THE WHITE PAVEMENT MARKINGS e.g. ASPHALT.
9. STREET LIGHTING SHOULD BE DESIGNED AND INSTALLED IN ACCORDANCE WITH A.S. 1158.
10. THE EFFECT THAT A RSP WILL HAVE ON HEAVY VEHICLES MUST BE CONSIDERED. THE FOLLOWING ARE KEY CONSIDERATIONS FOR HEAVY VEHICLES:
 - A. LOCATION & ORIENTATION OF THE APPROACH AND DEPARTURE RAMPS TO AVOID VEHICLE INSTABILITY.
 - B. MAXIMUM RSP HEIGHT TO AVOID VEHICLE INSTABILITY.
 - C. POTENTIAL OPERATIONAL DEFICIENCY AND DELAYS DUE TO THE LOWER ACCELERATION AND DECELERATION OF HEAVY VEHICLES.
 - D. POTENTIAL IMPLICATIONS OF HEAVY VEHICLE DRIVERS USING ALTERNATE ROUTES (E.G. LOCAL STREETS) TO AVOID THE RSP.
11. OTHER ROAD USERS SUCH AS BUSES, MOTORCYCLIST, CYCLISTS ETC. SHOULD BE CONSIDERED BASED ON INDIVIDUAL MERITS AND CONTEXT OF THE PROJECT.
12. THIS DRAWING HAS BEEN DEVELOPED TO REPRESENT A NEW SIGNALISED INTERSECTION. RETROFITTING RSP'S TO AN EXISTING SIGNALISED INTERSECTION MAY NOT BE POSSIBLE AND SHOULD BE ASSESSED ON A CASE BY CASE BASIS.
13. MR-WDO-1B SIGNS TO BE POSITIONED TO ENSURE THEY DO NOT MASK TRAFFIC SIGNAL ASPECTS. SIGN MAY BE INSTALLED ON TRAFFIC SIGNAL POLE IF VERTICAL CLEARANCES CAN BE ACHIEVED, MR-WDO-1A SIGN MAY BE USED IN THIS INSTANCE.



RAISED SAFETY PLATFORM
CONSTRUCTION DETAIL - SECTION A
N.T.S.

RAISED SAFETY PLATFORM
CONSTRUCTION DETAIL - SECTION B
N.T.S.

No.	DESCRIPTION	APPROVED & DATE
2	NOTE 13 AMENDED.	<i>Abomer 3/6/22</i>
1	PIANO KEY MARKINGS REPLACED BY SHARKS TEETH	E. ROSE 14/09/21
AMENDMENTS		

LEGEND

	RAISED SAFETY PLATFORM
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DRAWN	K. SMITH - AUGUST 2019
VERIFIED	R. KOORENGEVEL 26/09/2019
APPROVED	D. LANDMARK 26/09/2019
FILE NUMBER	

GUIDELINE DRAWING
 SIGNALISED INTERSECTION
RAISED SAFETY PLATFORM
 APPLICATION TYPE 2 - ENTIRE INTERSECTION RAISED
 LOCAL AUTHORITY
 MRWA DRAWING NUMBER
201931-0006-2
 AMENDMENT

NOT TO SCALE
SCALE A3