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Policy, Application and Technical Guidelines for Decorative Pavement Markings

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Document No: D25#858286

Issue Date: December 2025

Policy, Application and Technical Guidelines for Decorative Pavement Markings

This document is authorised by the Executive Director Network Operations.

Authorisation

As Executive Director Network Operations I authorise the issue and use of this document *Policy, Application and Technical Guidelines for Decorative Pavement Markings*

Approved by Executive Director Network Operations

Date:


MEHDI LANGROUDI
19 DEC 2025

All printed copies are uncontrolled.

All enquiries regarding this guideline should be directed to the Network Operations Planning Manager.

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Document control

Owner	Manager Traffic Management Services
Custodian	Network Operations Planning Manager
Document number	D25#858286
Issue date	December 2025
Review frequency	Every 5 years (or earlier if needed)

Amendments

Revision number	Revision date	Description of key changes	Section / page no.
0	May 2025	New policy, supersedes <i>Application and Technical Guidelines for Decorative Pavement Markings (Ver.1 – March 2019)</i>	All
1	December 2025	Policy updated to reflect feedback received from: <ol style="list-style-type: none"> 1. Main Roads Materials Branch (received 20/05/2025) 2. Main Roads Editorial Personnel (received 28/05/2025) 3. Department of Transport Urban Mobility Branch (received 10/06/2025) 4. WALGA (received 24/06/2025) 5. Main Roads Traffic Management Services (received 11/07/2025) 	Various

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1 Policy statement

A decorative pavement marking refers to a piece of artwork applied to the surface of a road, sometimes referred to in other jurisdictions as “roadway art”, “asphalt art”, or “road murals”.

Main Roads Western Australia **will support** the use of decorative pavement markings on roads where appropriate to enhance road safety, aesthetics and community identity.

2 Principles

The guidance and technical criteria outlined in this document have been established in line with the following principles.

2.1 Preserving the integrity of traffic control devices

Traffic Control Devices (TCDs) play a critical role in informing road users about required behaviours in a clear, unambiguous, and consistent manner. Over time, road users develop an automatic response to these devices, reinforcing a safe, efficient and reliable road system. To maintain compliance and ensure road safety, decorative pavement markings must not replicate, interfere with, or be integrated into TCDs.

2.2 Responding to movement and place

Movement and Place is a planning and design approach for roads and streets that recognises their dual roles: enabling the flow of people and goods (Movement) and functioning as quality destinations within the urban realm (Place). It emphasises consideration and seeking balance between these often-conflicting roles. Decorative pavement markings can contribute to the quality of a place.



Figure 1: Movement and place framework

Source: Austroads' Review of Movement and Place Guidance

For further information about Movement and Place, refer to [Austroads' Review of Movement and Place Guidance](#).

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2.3 Supporting the safe system approach to road safety

The safe system approach, comprised of safe roads, safe people, safe speeds, safe vehicles and post-crash care, is a method of road safety management based on the principle that human bodies can only withstand limited forces before injury or death occurs. The safe system approach has been adopted by all Australian jurisdictions to support road safety outcomes in jurisdictional and national road safety action plans. It underpins the *Driving Change – Road Safety Strategy for Western Australia (2020-2030)*.

The main objective of the safe system is to ensure, in the event of a crash, that the impact forces released are within the bounds of human tolerance, eliminating fatalities and serious injuries. The chances of surviving a crash decrease rapidly above certain impact speeds, depending on the nature of the collision.

Decorative pavement markings have emerged as a potential tool in supporting this approach, with growing evidence suggesting they can contribute to reducing vehicle speeds, thereby lowering crash risk and severity. This effect is particularly notable in highly urbanised environments where the “place” function of roads is significant. By visually influencing driver behaviour, decorative pavement markings can help create safer road environments while also enhancing the streetscape and reinforcing local identity.

2.4 Contributing to public art

Main Roads acknowledges that art in public places can be used to enhance local identity and to promote a sense of place. The number of proposals for incorporating public art into the road reserve is increasing, with the community generally accepting these initiatives as making a positive contribution to the built environment. Traditionally, public art has been incorporated onto vertical surfaces such as noise walls and bridge abutments, however the concept of applying artwork to pavement surfaces has gained popularity in recent years.



Figure 2: South Fremantle Bike Hub by Town Team Movement and RAC

Source: Town Team Movement

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3 Purpose

This document provides practitioners with technical guidance surrounding the location, design and installation of decorative pavement markings. Additionally, it confirms the roles and responsibilities of Main Roads and local government with respect to reviews, approvals and consultation.

4 Scope

4.1 Objectives

The objectives of decorative pavement markings include:

- increasing road user attentiveness and encouraging lower vehicle speeds
- highlighting to motorists that they are entering a different environment with increased pedestrian activity
- promoting active modes of transport such as walking and cycling
- facilitating street beautification and placemaking, resulting in increased urban vibrancy
- enabling communities to participate in shaping the look and feel of their local built environment.

4.2 Inclusions and exclusions

These guidelines only apply to the application of decorative pavement markings on roads.

These guidelines do not apply to markings on paths nor to artwork applied to walls, fences, or other vertical surfaces.

Graffiti and other forms of illegal or unapproved artwork do not form part of the scope of this document.

While decorative pavement markings are typically created using paint or paint products, the technical criteria outlined in this document also apply to road surface artwork created using pavers, mosaics, decals, or other similar mediums.

5 Roles and responsibilities

These guidelines apply to all **local and State roads** in Western Australia.

When proposing decorative pavement markings on **local roads**, applicants must liaise with their relevant local government who in-turn may be required to refer the application to Main Roads for review and approval.

When proposing decorative pavement markings on **State roads**, applicants must liaise directly with Main Roads, per Table 1.

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Table 1: Review and approval of Decorative Pavement Marking

Role	Responsibility
Manager Traffic Management Services or Regional Director	Approval of installation, modification or removal of Decorative Pavement Markings
Traffic Services Manager	Review of installation, modification or removal of Decorative Pavement Markings
Traffic Services Coordinator	Recommendation for installation, modification or removal of Decorative Pavement Markings

5.1 Review and Approvals

The process surrounding the review and approval of decorative pavement marking schemes depends on the ownership of the road in question, and the proposed decorative pavement marking's proximity to an intersection, regulatory road marking or other traffic control device.

For decorative pavement markings on **Local Roads**, applicants must first liaise with their relevant local government who in-turn may be required to refer the application to Main Roads for review and approval.

For decorative pavement markings on **State Roads**, applicants must liaise directly with Main Roads. Refer to

Figure 3 and Figure 4 for more information.

It should be noted that if issues are identified with the proposed design or location of a decorative pavement marking, then Main Roads will provide feedback regarding potential modifications or alternative locations.

In Figure 3 and Figure 4:

- **Green zones** are defined as locations where decorative pavement markings are generally supported.
- **Amber zones** are defined as locations where decorative pavement may potentially be supported but require referral to Main Roads to ensure they do not compromise road safety.
- **Red zones** are defined as locations where decorative pavement markings are not permitted under any circumstances.

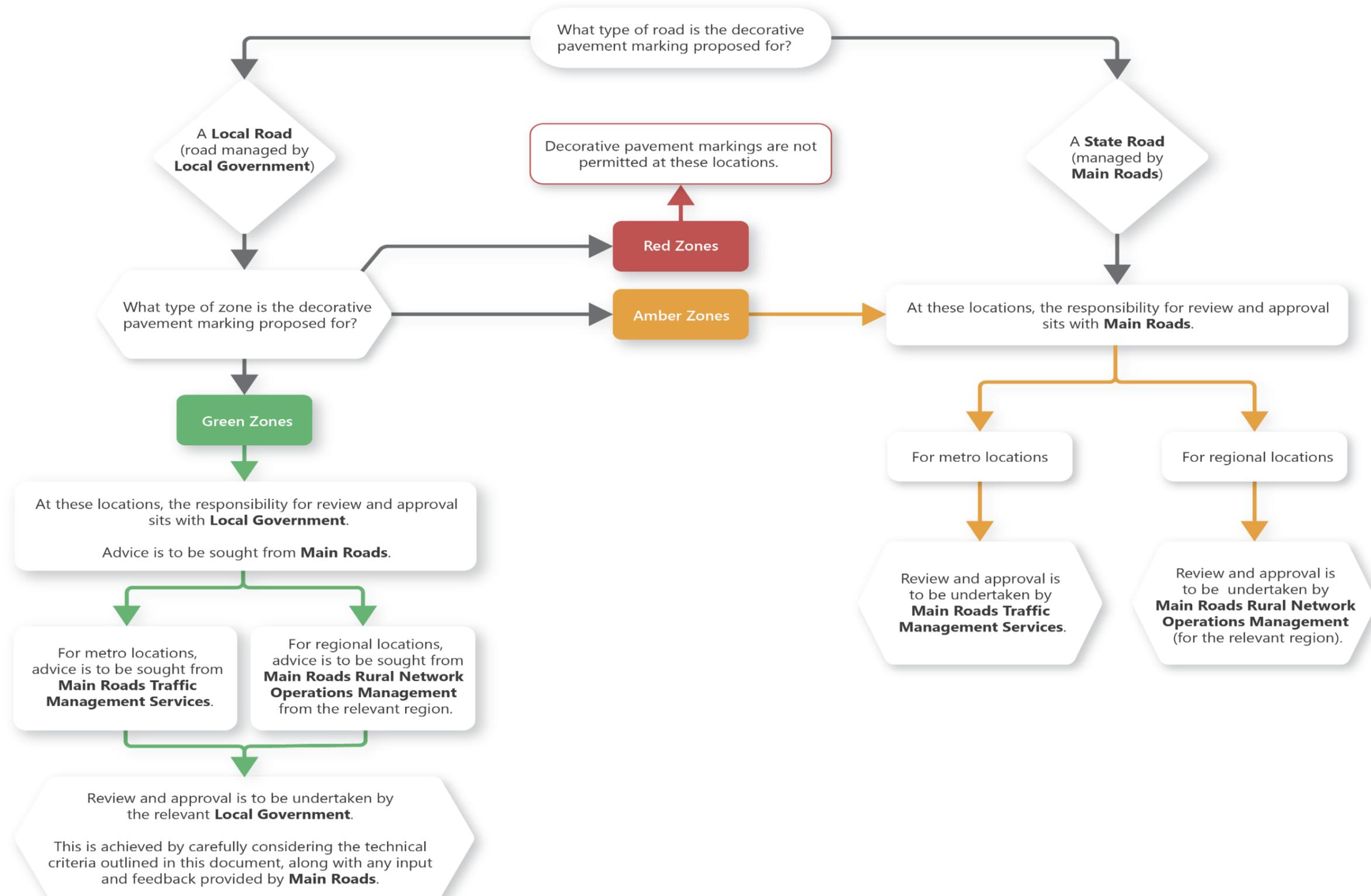


Figure 3: Review and approvals process for decorative pavement markings

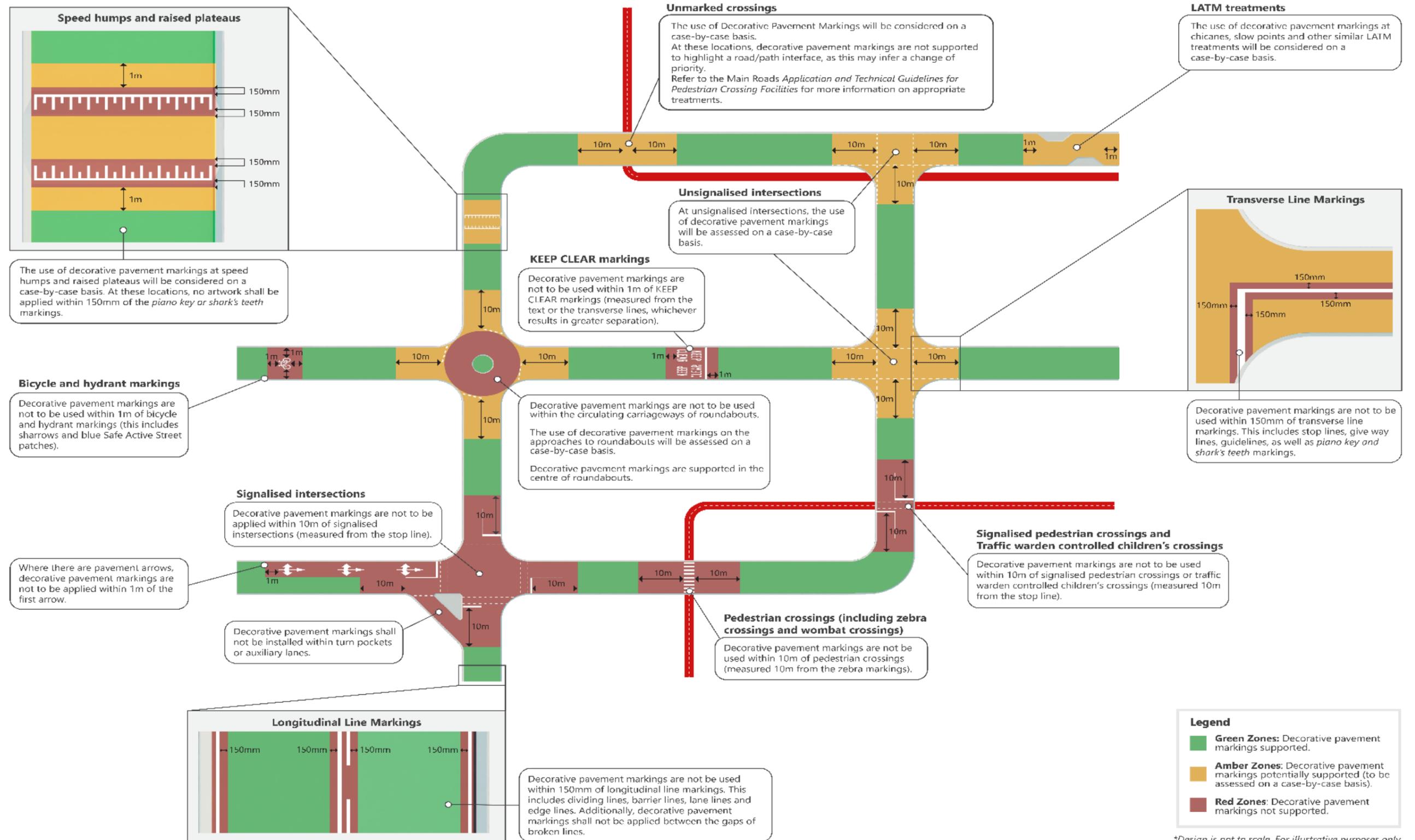


Figure 4: Decorative pavement marking exclusion zones

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6 Definitions

The following definitions, abbreviations and acronyms apply in this guideline:

Table 2: Definitions

Term	Definition
Decorative pavement marking (DPM)	A decorative pavement marking refers to a piece of artwork applied to surface of a road. Decorative pavement markings have no legal status and are not intended to be used as regulatory traffic control devices. Terminologies used in other jurisdictions include "roadway art", "asphalt art" and "road murals".
Fire Hydrant	As defined in the <i>Road Traffic Code 2000</i> as "an upright pipe with a spout, nozzle or other outlet for drawing water from a main or service pipe in case of fire or other emergency"
FSI crash	An FSI crash refers to a road traffic crash that results in a fatality or serious injury.
Exclusion zone	An exclusion zone is an area where decorative pavement markings are prohibited. It is typically based on proximity to a traffic control device.
LATM	Local Area Traffic Management
Local road	A local road in Western Australia is a road that is managed and maintained by local government. Local roads are distinct from State roads, which are managed by Main Roads.
Main Roads	Main Roads Western Australia
Mid-block	A road environment located outside of the functional area of an intersection.
Mid-block signalised pedestrian crossing	A pedestrian crossing controlled by traffic signal, not at an intersection.
Road Safety Audit (RSA)	In Western Australia, a Road Safety Audit (RSA) is a formal process conducted to evaluate the safety performance of road projects. The aim is to identify potential safety issues and recommend measures to mitigate them, thereby improving the overall safety for all road users.
Shared zone	Area or length of road used by both vehicles and pedestrians with a speed limit of either 10 km/h or 20 km/h.
State road	Under Section 6 of the <i>Main Roads Act 1930</i> , a road proclaimed to be highway or main road.
Traffic control device (TCD)	Any sign, signal, pavement marking or other installation placed or erected by a public authority or official body, having the necessary jurisdiction, for the purpose of regulating, warning or guiding traffic. This includes traffic signals, road signs, pavement markings and other devices used to manage traffic flow and ensure road safety.
Wombat crossing	A wombat crossing is a pedestrian crossing that combines a raised safety platform (or speed table) with zebra crossing markings on top of the platform.
Zebra crossing	The colloquial term for a pedestrian crossing delineated by white stripes approximately parallel to the centre line of the roadway, where pedestrians have priority over vehicular traffic under the road rules.

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7 Policy application and technical guidelines

This section provides guidance to practitioners around the location, artwork design and materials criteria that should be considered when implementing decorative pavement markings. Decorative pavement markings must not compromise the visibility of traffic control devices (TCDs).

Notwithstanding compliance with all technical requirements specified in this document, Main Roads will not support decorative pavement markings if they compromise road safety.

7.1 Location and placement criteria

7.1.1 Crash history

When considering the implementation of decorative pavement markings, proponents should assess the crash history of a location to ensure that safety is not compromised. Refer to the Main Roads [Crash Map](#) for more information. Sites with a significant history of crashes, particularly those involving fatalities or serious injuries (FSI), require careful evaluation to determine whether decorative pavement markings are appropriate.

It should be noted that decorative pavement markings are not an official crash reduction treatment. For information on commonly used road safety treatments and countermeasures, practitioners should refer to the Main Roads [ROSMA Treatment Resource Guide](#).

7.1.2 Suitable road environments

The use of decorative pavement markings is typically only supported on local access roads, shared zones, laneways, and other low-speed road environments.



Figure 5: Roxy Lane by Town Team Movement and RAC

Source: Town Team Movement

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The appropriateness of using decorative pavements markings in different speed environments is defined in Table 3 below.

Table 3: Appropriateness of decorative pavement markings based on posted speed limits

Posted speed limit	Appropriateness
≤ 50 km/h	High merit
60 km/h	Low merit
≥ 70 km/h	No merit

The appropriateness of using decorative pavements markings based on lane configuration is defined in Table 4 below.

Table 4: Appropriateness of decorative pavement markings based on lane configuration

Lane configuration	Appropriateness
One lane per direction	Some merit
Two lanes per direction	Low merit
Three or more lanes per direction	No merit

The appropriateness of using decorative pavements markings based on movement and place typologies is defined in Table 5 below.

Table 5: Appropriateness of decorative pavement markings based on movement and place typology

Movement and place typologies	Appropriateness
Low movement, high place	High merit
Low movement, low place	Low merit
High movement, high place	Low merit
High movement, low place	No merit

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7.1.3 Proximity to other road features

Careful consideration must be given to ensure that decorative pavement markings do not compromise road user safety, or the effectiveness of existing traffic control devices. The following criteria should be read in conjunction with the diagram provided in Figure 4.

General

- Decorative pavement markings must not mask or obscure any regulatory pavement markings.

Signalised intersections

- Decorative pavement markings are not to be applied within 10m of signalised intersections (measured from the stop line).
- Decorative pavement markings shall not be installed within turn pockets or auxiliary lanes.
- Where there are pavement arrows, decorative pavement markings are not to be applied within 1m of the first arrow.

Roundabouts

- Decorative pavement markings are not to be used within the circulating carriageways of roundabouts.
- The use of decorative pavement markings on the approaches to roundabouts will be considered by Main Roads on a case-by-case basis.
- Decorative pavement markings are supported in the non-trafficable centre of roundabouts.

Unsignalised intersections

- The use of decorative pavement markings at unsignalised intersections will be considered by Main Roads on a case-by-case basis.

Signalised pedestrian crossings and traffic warden-controlled children's crossings

- Decorative pavement markings are not to be used within 10m of signalised pedestrian crossings or traffic warden-controlled children's crossings (measured 10m from the stop line).

Pedestrian crossings (including zebra crossings and wombat crossings)

- Decorative pavement markings are not to be used within 10m of pedestrian crossings (measured 10m from the zebra markings).

Speed humps and raised plateaus

- The use of decorative pavement markings at speed humps and raised plateaus will be considered by Main Roads on a case-by-case basis.
- At these locations, no artwork shall be applied within 150mm of the *piano key* or *shark's teeth* markings.

LATM treatments

- The use of decorative pavement markings at chicanes, slow points and other similar LATM treatments will be considered by Main Roads on a case-by-case basis.

Unmarked crossings

- The use of Decorative Pavement Markings will be considered on a case-by-case basis.

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- At these locations, decorative pavement markings are not supported to highlight a road/path interface, as this may infer a change of priority.
- Refer to the Main Roads *Application and Technical Guidelines for Pedestrian Crossing Facilities* for more information on appropriate treatments.

Longitudinal Line Markings

- Decorative pavement markings are not to be used within 150mm of longitudinal line markings.
- This includes dividing lines, barrier lines, lane lines and edge lines.
- Additionally, decorative pavement markings shall not be applied between the gaps of broken lines.

Transverse Line Markings

- Decorative pavement markings are not to be used within 150mm of transverse line markings.
- This includes stop lines, give way lines, guidelines, as well as piano and shark's teeth markings.

Kerbing

- Decorative pavement markings are not to be used within 150mm of kerbing, to ensure clear delineation between the road and adjacent paths / verge areas.

RRPMs

- Decorative pavement markings are not to be used within 150mm of retroreflective raised pavement markers (RRPMs), also known as "cat's eyes".

Bicycle and hydrant markings

- Decorative pavement markings are not to be used within 1m of bicycle and hydrant markings (this includes sharrows and blue Safe Active Street patches).

KEEP CLEAR markings

- Decorative pavement markings are not to be within 1m of keep clear markings (measured from the text or the transverse lines, whichever results in greater separation).

7.2 Artwork design criteria

7.2.1 Colours, shapes and patterns

The design of decorative pavement markings must not create confusion for road users or pose accessibility challenges. The thoughtful selection of colours, shapes and patterns is essential to maintain road safety and compliance with traffic control devices.



Figure 6: Terrace by Town Team Movement, RAC and South Hedland Follow the Dream students

Source: Town Team Movement

- Decorative pavement markings must not create confusion or difficulty for people who have a cognitive or intellectual disability, a vision-impairment or identify as neurodiverse. Consultation with relevant community groups is encouraged during the conceptualisation phase (refer to Section 8.2).
- Colours used in decorative pavement markings must not interfere with or resemble traffic control devices, including white or yellow lines.
- Geometric shapes that could be mistaken for traffic control devices (such as red octagons resembling stop signs) or warning signs (such as yellow diamonds or squares) must not be incorporated into decorative pavement markings. Additionally, shapes resembling arrows must not be used.
- The use of red, yellow or green circles must not be used to avoid confusion with traffic signals.
- Decorative pavement markings must not resemble playground games (hopscotch, foursquare,) or sports field markings (football, basketball).

7.2.2 Readability and appropriate content

Decorative pavement markings must be designed to ensure clarity, immediate recognition and minimal cognitive load for road users. Complex or misleading elements are to be avoided to maintain road safety.

Decorative pavement markings **must be**:

- recognisable “at a glance”
- easy to understand and interpret without significant cognitive effort
- designed and oriented to ensure they do not suggest incorrect lane use or wrong-way travel

Decorative pavement markings **must not**:

- contain letters or numbers
- contain any directional or wayfinding information
- contain any symbols that are used for traffic management purposes (bicycle symbols, pedestrian symbols) as described in AS 1742.
- include any commercial messaging, advertising or company logos
- feature any content that is salacious, illegal or of a controversial nature
- attempt to simulate three-dimensional objects or optical illusions.

7.3 Material specifications criteria

The materials used for decorative pavement markings needs to maximise safety, durability and visual effectiveness. Only materials listed below may be used:

- Decorative pavement markings need to be constructed from durable materials that maximise adhesion and maintain colour retention over time.
- Decorative pavement markings need to be skid and slip-resistant to prevent hazards for all road users, including pedestrians and cyclists (≥ 45 BPN or equivalent when measured in accordance with WA 310.1).
- Decorative pavement markings must not incorporate retroreflective properties to avoid confusion with regulatory pavement markings.
- Photoluminescent (“glow-in-the-dark”) materials must not be used in decorative pavement markings.
- To prevent accelerated pavement deterioration, decorative pavement markings must not generate or trap additional surface heat.
- Decorative pavement markings need to be created using products approved by the Australian Paint Approval Scheme (APAS) for pavement marking applications.
- Decorative pavement markings need to be created from non-toxic paint types. The choice of cold applied plastics, 2-pack epoxies, urethanes, resin bonded to pre-coloured aggregate, water-based or solvent free paint, should reflect local environmental conditions.

8 Consultation, monitoring and evaluation

8.1 Consultation with road asset owner

Early engagement needs to occur between the applicant and the road asset owner (local government or Main Roads) to establish whether there is in-principal support for the decorative pavement marking project. Evidence of this engagement must be submitted as part of the application checklist (refer to Appendix A).

As part of this process, the road asset owner should consider the timing of any upcoming resurfacing/reconstruction works, or whether any existing nearby line marking needs to be removed, updated or rectified.

8.2 Consultation with other stakeholders

Applicants are encouraged to engage with the community and relevant stakeholders when planning decorative pavement marking projects. Engagement with stakeholder is mandatory and may include, but not limited to:

- nearby businesses and adjoining landowners to obtain their support and address any concerns
- disability advocacy groups to ensure proposed designs do not create confusion or safety risks
- local schools and community groups to contribute ideas for the artwork design to promote community ownership and involvement
- the Public Transport Authority in situations where a decorative pavement marking is near a bus stop, bus embayment or transit lane.

Evidence of this engagement must be provided to the road asset owner as part of the application checklist (refer to Appendix A).

8.3 Before and after traffic surveys

As part of decorative pavement marking projects, the road asset owner is encouraged to undertake before-and-after traffic surveys.

The objectives of these surveys include:

- evaluating changes in median and 85th percentile speeds
- evaluating impacts on traffic volumes
- assessing any changes in road user behaviour and crash propensity.

Post-implementation surveys are recommended to take place 12 months after installation to monitor long-term effects and inform future applications.

8.4 Condition assessments

Decorative pavement markings can be subject to erosion, fading or discoloration particularly in heavily trafficked locations.

Regular condition assessments (refer AS 4049.3) should be undertaken by the road asset owner. When signs of deterioration emerge, steps should be put in place to remediate or remove the decorative pavement marking.

8.5 Removal of decorative pavement markings

The triggers for removing decorative pavement markings may include artwork deterioration, road resurfacing, utilities installation or road network changes (for example, if an unsignalised intersection is converted to roundabout).

The road asset owner and/or approving authority may direct the removal of decorative pavement marking should non-compliance be identified.

The costs associated with removal of the decorative pavement marking will be negotiated between the applicant and the road asset owner. Maintenance and removal responsibilities including costs associated with the removal must be agreed prior to approval as part of Maintenance Plan. A Maintenance Plan must outline:

- Inspection frequency
- Remediation triggers
- Removal procedures and cost sharing arrangements.

Main Roads will not reinstate Decorative Pavement Markings after resurfacing or reconstruction works.

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9 References

Table 6: References to legislation, regulations and standards

Document number	Description
AS 1742	Manual of Uniform Traffic Control Devices
AS 1158	Lighting for Roads and Public Spaces
AS 4049	Paints and Related Materials – Road Marking Materials
WA 310.1	Pavement Skid Resistance: British Pendulum Method
RTC2000	Road Traffic Code 2000
Main Roads Act (1930)	The Act sets out the provision for classification, construction, maintenance and supervision of roads

Table 7: References to policies and guidelines

Document number	Description
N/A	Main Roads Specification 604 Pavement Marking
N/A	Road Safety Management at Main Roads (ROSMA) – Policy
AGTM4	Austrroads Guide to Traffic Management Part 4: Network Management Strategies.
AP-R560-18	Austrroads Towards Safe System Infrastructure: A Compendium of Current Knowledge (2018)
D18#356205	Main Roads Skid Resistance Management Plan
N/A	Bring Your Street Alive, Town Team Movement
N/A	Safer Speeds & Better Places Community Toolkit, Town Team Movement
D24#590607	Application and Technical Guidelines for Pedestrian Crossing Facilities

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Appendix A: Application checklist

When proposing decorative pavement markings on **State Roads**, applicants must compile the information listed in Table 8 and submit this to Main Roads for review and consideration.

When proposing decorative pavement markings on Local Roads, applicants are encouraged to compile the information listed in Table 8 and submit this to the relevant local government for review and consideration. This information should then be forwarded to Main Roads as part of the advice or referral process described in

Figure 3.

Table 8: Application checklist

Item	Information required	Check if complete ✓
1	Provide an overview of the proposed decorative pavement marking project summarising its historical context and the objectives it is seeking to achieve.	
2	Attach relevant drawings or sketches of the planned decorative pavement marking, which confirm the proposed location, size and artwork design.	
3	Provide evidence and commentary on how the proposed decorative pavement marking complies with the location and placement criteria outlined in Section 7.1 of this guideline.	
4	Provide evidence and commentary on how the proposed decorative pavement marking complies with the artwork design criteria outlined in Section 7.2 of this guideline.	
5	Provide evidence and commentary on how the proposed decorative pavement marking complies with the material specifications criteria outlined in Section 7.3 of this guideline.	
6	Provide details of any stakeholder engagement undertaken to date, including correspondence with the relevant road network asset owner (local government or Main Roads).	
7	Provide information surrounding the site's existing traffic volumes, vehicles speeds and crash history.	
8	Provide details of your monitoring and evaluation plan, detailing: <ul style="list-style-type: none"> the expected lifespan of the decorative pavement marking how often the decorative pavement marking will be inspected plans for maintenance, removal or decommissioning of the decorative pavement marking. 	

Appendix B: Example locations

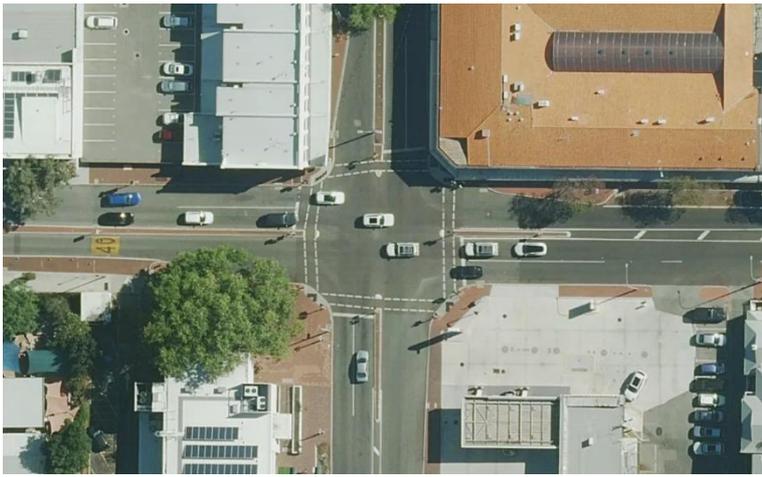
Table 9: Example Locations

Location	Status	Explanation / supporting commentary	Example
Laneways	Typically supported	Urban laneways are generally considered suitable for decorative pavement markings.	
Urban high streets	Typically supported	Urban high streets are generally considered suitable for decorative pavement markings. Mid-block locations are considered more appropriate than intersections.	
Residential streets	Typically supported	Residential streets are generally considered suitable for decorative pavement markings. Mid-block locations are considered more appropriate than intersections.	

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Location	Status	Explanation / supporting commentary	Example
Unsignalised intersections	Potentially supported (assessed on case-by-case basis)	Decorative pavement markings at unsignalised intersections will be considered on a case-by-case basis.	
In combination with raised plateaus or other LATM devices	Potentially supported (assessed on case-by-case basis)	<p>The use of decorative pavement markings at speed humps and raised plateaus will be considered on a case-by-case basis.</p> <p>At these locations, no artwork shall be applied within 150mm of the piano key or shark's teeth markings.</p>	
Road/path interfaces	Potentially supported (assessed on case-by-case basis)	<p>The use of Decorative Pavement Markings will be considered on a case-by-case basis.</p> <p>At these locations, decorative pavement markings are not supported to highlight a road/path interface, as this may infer a change of priority.</p>	

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Location	Status	Explanation / supporting commentary	Example
Roundabouts	Potentially supported (assessed on case-by-case basis)	<p>Decorative pavement marking in the centre of roundabouts is supported.</p> <p>Decorative pavement marking on approaches to roundabouts is to be assessed on a case-by-case basis.</p> <p>Decorative pavement markings are not to be used within the circulating carriageways of roundabouts.</p>	
Signalised intersections	Not supported	<p>Decorative pavement markings are not to be applied within 10m of signalised intersections (measured from the stop line).</p>	
Signalised pedestrian crossings and Traffic warden-controlled children's crossings	Not supported	<p>Decorative pavement markings are not to be used within 10m of a signalised pedestrian crossings or at traffic warden-controlled children's crossings (10m from the stop line).</p>	

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Location	Status	Explanation / supporting commentary	Example
Pedestrian crossings	Not supported	Decorative pavement markings are not to be used within 10m of pedestrian crossings (measured 10m from the zebra markings).	
Keep Clear	Not supported	Decorative pavement markings must not be used within 10 metres of the Keep Clear markings.	
Pavement arrows	Not supported	Decorative pavement markings are not to be used in locations where there are pavement arrows. Where there are pavement arrows, decorative pavement markings are not to be applied within 1m of the first arrow.	

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Location	Status	Explanation / supporting commentary	Example
Auxiliary lanes or turning pockets	Not supported	Decorative pavements markings must not be used within auxiliary lanes or turning pockets.	
Road/path interfaces	Not supported	Decorative pavement markings are not supported to highlight a road/path interface, as this may infer a change of priority. Refer to the Main Roads <i>Application and Technical Guidelines for Pedestrian Crossing Facilities</i> for more information on appropriate treatments.	

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